

## Chapter 20.280

### DOWNTOWN STATION AREA SPECIFIC PLAN DISTRICT

#### Sections:

- 20.280.001 Purpose**
- 20.280.002 Sub-Districts**
- 20.280.003 Land Use Regulations**
- 20.280.004 Development Standards**
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#### **20.280.001 Purpose**

This chapter establishes regulations for the Downtown Station Area Specific Plan (DSASP) Zoning District. The Downtown Station Area Specific Plan District applies to land within the Downtown Station Area Specific Plan. This district is intended to establish the use regulations, standards and development review procedures needed to implement the Downtown Station Area Specific Plan. For purposes of design review, the Design Guidelines in the Downtown Station Area Specific Plan shall be used.

#### **20.280.002 Sub-Districts**

The following sub-districts are established within the Downtown Station Area Specific Plan District. Sub-district boundaries are shown on the official Zoning Map. The sub-districts west of US 101 are referred to as the “Downtown” and the sub-districts located east of US 101 are referred to as the “Eastern Neighborhood.” The purpose of each sub-district is as follows:

A. **Downtown Transit Core (DTC).** The Downtown Transit Core sub-district is focused within a ¼ radius of a point just east of US 101 at East Grand Avenue. This location corresponds to the planned extension of the Caltrain Station and accompanying pedestrian/bicycle rail undercrossing. It is intended to provide sites for mixed-use development at high intensities in proximity to the Caltrain Station. It encourages active ground floor uses and high intensity development that will generate pedestrian traffic in the area. The Downtown Transit Core sub-district flanks the Grand Avenue Core sub-district which will be the epicenter of Downtown commercial uses. The Downtown Transit Core District, will provide additional population and activities to support Grand Avenue uses, increase Caltrain transit ridership, and provide housing with high amenity value for new residents.

B. **Grand Avenue Core (GAC).** The Grand Avenue Core sub-district extends along Grand Avenue from Airport Boulevard on the east to Spruce Avenue on the west. It is intended to be the “main street” of the Downtown. This sub-district will build on the history and layout of the old Downtown while providing additional capacity for residential and commercial uses. Located directly west of the relocated Caltrain Station, the rail undercrossing, and the East of 101 Office/R&D District, the Grand Avenue Core sub-district will benefit from improved accessibility to employment east of the rail line as well as increased residential populations in the Downtown. The Grand Avenue Core sub-district is intended to provide a nearly continuous retail frontage with high density residential above, although at somewhat lower densities than the adjoining Downtown Transit Core district.

C. **Downtown Residential Core (DRC).** The Downtown Residential Core sub-district is located in the Downtown in the remainder of the Pedestrian Priority Zone not defined as Grand Avenue Core or

Downtown Transit Core. This sub- district is defined by Tamarack Lane on the north, Second Lane on the south, Spruce Avenue on the west, and Airport Boulevard on the east. This district is intended to provide for high density residential neighborhoods near the center of Downtown and within about ½ mile of the Caltrain Station. This sub-district allows, but does not require commercial uses on the ground floor. This sub-district will provide additional residential opportunities within an area that will have significant pedestrian and bicycle improvements to allow easy access to Caltrain and the employment center east of US 101.

**D. Transit Office / R&D Core (TO/RD).** The Transit Office/R&D Core sub-district is located just east of the Caltrain tracks in an area bounded by East Grand Avenue on the north, Gateway Boulevard on the east, Poletti Way and US 101 on the west, and S. Airport Boulevard on the south. This sub-district is intended to provide a location for the highest intensity office or R&D uses. Suited to headquarters or other office type uses that do not include significant manufacturing, the sub-district offers the opportunity for locating high intensity uses in immediate proximity to the Caltrain Station. In addition, with the relocation of the Caltrain Station and construction of a pedestrian and bicycle rail undercrossing, this sub-district will provide convenient access to Grand Avenue and the surrounding areas and will support commercial revitalization.

**E. Linden Neighborhood Center (LNC).** The Linden Neighborhood Center is located north of Grand Avenue on Linden Avenue between Ninth Lane and California Avenue. This sub-district includes some existing local-serving businesses which will form the foundation for a cluster of retail, services and amenities that can serve the surrounding residential neighborhoods. The Linden Neighborhood Center designation will encourage mixed use development, with retail uses at the ground floor and residential above.

**F. Linden Commercial Corridor (LCC).** The Linden Commercial Corridor lies north of Grand Avenue between California Avenue and Sixth Lane, and south of Grand Avenue between Second Lane and Railroad Avenue. This sub-district will allow but will not require ground level mixed use, neighborhood-serving and retail uses within otherwise residential or commercial areas.

**G. Other Downtown Districts.** See Downtown Districts chapter (20.100) and Employment Districts (20.110) for information on unchanged land use districts found in the Specific Plan and the larger Downtown area, including Downtown Residential High, Downtown Mixed Use, Downtown Residential Medium, Downtown Residential Low, Business Commercial, and Mixed Industrial.

### **20.280.003 Land Use Regulations**

Table 20.280.003 below prescribes the land use regulations for “Downtown Station Area Specific Plan” sub-districts. The regulations for each sub-district are established by letter designations as follows:

“P” designates permitted uses.

“MUP” designates use classifications that are permitted after review and approval of a Minor Use Permit by the Chief Planner.

“C” designates use classifications that are permitted after review and approval of a Conditional Use Permit by the Planning Commission.

“(#)” numbers in parentheses refer to specific limitations listed at the end of the table.

“-” designates uses that are not permitted.

Use classifications are defined in Chapter 20.620 (“Use Classifications”). In cases where a specific land use or activity is not defined, the Chief Planner shall assign the land use or activity to a classification that is substantially similar in character. Use classifications and subclassifications not listed in the table or not found to be substantially similar to the uses below are prohibited. The table also notes additional

use regulations that apply to various uses. Section numbers in the right hand column refer to other sections of this title.

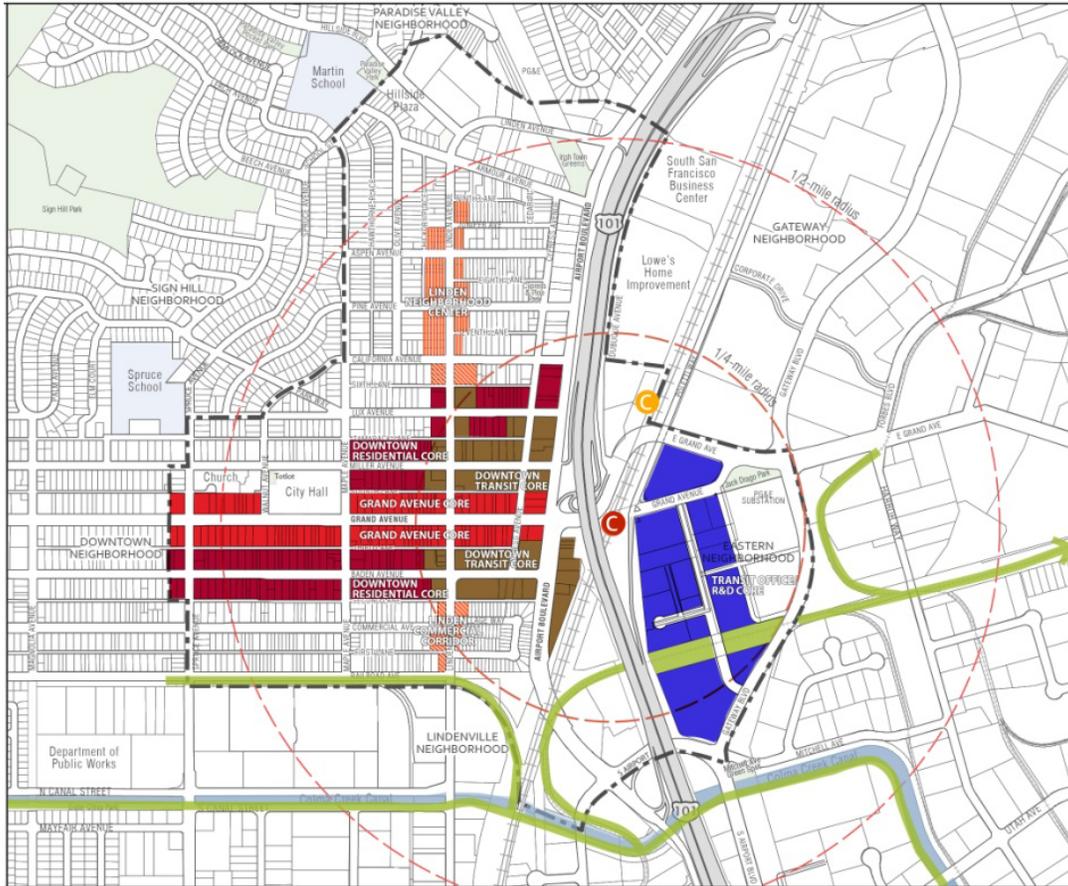
**Table 20.280.003  
Land Use Regulations Downtown Station Area Specific Plan Sub-Districts**

Uses Permitted	DTC	GAC	DRC	TO/RD	LCC	LNC	Additional Regulations
<b>Residential Use Classifications</b>							
Single-Unit Dwelling	<i>See sub-classifications below</i>						
<i>Single Unit Detached</i>	(1)	(1)	(1)	-	(1)	(1)	
<i>Second Unit</i>	(1)	(1)	(1)	-	(1)	(1)	See Section 20.350 Second Dwelling Units
<i>Single Unit Semi-Attached</i>	(1)	(1)	(1)	-	(1)	(1)	
<i>Single-Unit Attached</i>	(1)	(1)	(1)	-	(1)	(1)	
Multiple-Unit Residential	<i>See sub-classifications below</i>						
<i>Duplex</i>	(1)	-	(1)	-	(1)	(1)	
<i>Multi-Unit</i>	P	P (3)	P (3)	-	P (3)	P (3)	
<i>Senior Citizen Residential</i>	P	-	P	-	P (3)	P (3)	
Domestic Violence Shelter	-	-	P (2)	-	-	-	See Section 20.350 Domestic Violence Shelter
Family Day Care Home	<i>See sub-classifications below</i>						
<i>Large</i>	-	-	P	-	-	-	See Section 20.350 Family Day Care, Large
<i>Small</i>	P	-	P	-	P	P	
Group Residential	-	-	C	-	-	-	See Section 20.350 Group Residential Facilities
Residential Care Facilities	<i>See sub-classifications below</i>						
<i>General</i>	C (4)	-	C (4)	-	-	-	See Section 20.350 Group Residential Facilities
<i>Limited</i>	C (4)	(1)	C (4)	-	(1)	(1)	
<i>Senior</i>	MUP (4)	(1)	MUP (4)	-	C (3)	C (3)	See Section 20.350 Group Residential Facilities
Single Room Occupancy	(1)	(1)	(1)	-	(1)	(1)	
<b>Public and Semi-Public Use Classifications</b>							
Colleges and Trade Schools, Public or Private	C	C	C	C	C (3)	C (3)	
Community Assembly, 2000 Square Feet or Less	-	-	C	-	C (3)	C (3)	See Section 20.350 Community Assembly Facilities
Community Assembly, More Than 2000 Square Feet	-	-	-	-	C (3)	C (3)	See Section 20.350 Community Assembly Facilities
Community Garden	-	-	P	-	P	P	
Cultural Institutions	C	C	C	C	C	C	
Day Care Centers	P	-	P	C	-	-	See Section 20.350 Day Care Centers
Government Offices	P	P	P	P	P	P	

Uses Permitted	DTC	GAC	DRC	TO/RD	LCC	LNC	Additional Regulations
<b>Public and Semi-Public Use Classifications (cont'd)</b>							
Hospitals and Clinics	<i>See sub-classifications below</i>						
<i>Clinics</i>	-	MUP (7)	-	-	MUP	MUP	See Section 20.350 Clinics in Downtown Core
Park and Recreation Facilities, Public	P	P	P	P	P	P	
Public Safety Facilities	P	P	P	P	P	P	
Social Service Facilities	P (6)	-	-	-	P (6)	P (6)	See Section 20.350 Social Service Facilities
<b>Commercial Use Classifications</b>							
Animal Care, Sales and Services	<i>See sub-classifications below</i>						
<i>Pet Stores</i>	P	P	P	-	P	P	See Section 20.350 Animal Care, Sales, and Services
<i>Veterinary Services</i>	C	-	C	-	C	C	See Section 20.350 Animal Care, Sales, and Services
Artists Studios	P	P	C	-	P	P	
Banks and Financial Institutions	<i>See sub-classifications below</i>						
<i>Banks and Credit Unions</i>	P	MUP	-	P	MUP	MUP	
Business Services	P	P (3)	-	P	P	P	
Commercial Entertainment and Recreation	<i>See sub-classifications below</i>						
<i>Indoor Entertainment</i>	MUP	C	-	-	C	C	
<i>Indoor Sports and Recreation</i>	MUP	C (4)	-	C	C	C	
Eating and Drinking Establishments	<i>See sub-classifications below</i>						
<i>Coffee Shops/Cafés</i>	P	P	P	P	P	P	See Section 20.350 Outdoor Seating
<i>Restaurants, Full Service</i>	P	P	C	P	P	P	See Section 20.350 Outdoor Seating
<i>Restaurants, Limited Service</i>	P	P	P	P	P	P	Subject to approved Trash Management Plan
<i>Convenience Market</i>	P	P	P	P	P	P	See Section 20.350 Convenience Market
<i>Grocery Store</i>	P	P	C	-	P	P	
<i>Supermarket</i>	P	-	-	-	C	C	
Funeral Parlors and Mortuaries	-	-	-	-	-	-	
Lodging	<i>See sub-classifications below</i>						
<i>Bed and Breakfast</i>	-	MUP (5)	C	-	MUP	MUP	See Section 20.350 Bed and Breakfast Lodging
<i>Hotels and Motels</i>	C	C (5)	-	C	-	-	
Maintenance and Repair Services	P	-	-	-	P	P	

Uses Permitted	DTC	GAC	DRC	TO/RD	LCC	LNC	Additional Regulations
<b>Commercial Use Classifications (cont'd)</b>							
Offices	<i>See sub-classifications below</i>						
<i>Business and Professional</i>	P	P (3)	MUP	P	P (3)	P (3)	
<i>Medical and Dental</i>	P	P (3)	MUP	P	P (3)	P (3)	
<i>Walk-In Clientele</i>	P	P (3)	MUP	P	P	P	
Parking, Public or Private	P	-	P	P	P	P	
Personal Services	<i>See sub-classifications below</i>						
<i>General Personal Services</i>	P	P	P	P	P	P	Section 20.350 Personal Services
Retail Sales	<i>See sub-classifications below</i>						
<i>General Sales</i>	P	P	P	P	P	P	
<i>Secondhand Store</i>	C	C	P	-	P	P	
<b>Employment Use Classifications</b>							
Clean Technologies	MUP	-	-	P	-	-	
Handicraft/Custom Manufacturing	MUP	-	-	P	-	-	
Research and Development	MUP	-	-	P	-	-	
<b>Transportation, Communication, and Utilities Use Classifications</b>							
Utilities, Major	-	-	-	C	-	-	
Utilities, Minor	P	P	P	P	P	P	
<b>Other Applicable Use Regulations</b>							
Accessory Uses	[See Section 20.300.002 Accessory Buildings and Structures]						
Home Occupations	P	P	P	-	P	P	See Section 20.350 Home Occupations
Nonconforming Use	See Chapter 20.320 Nonconforming Uses, Structures and Lots						
Temporary Use	See Chapter 20.340 Temporary Uses						
<b>Limitations:</b>							
<ol style="list-style-type: none"> <li>1. Permitted if existing. New units not allowed.</li> <li>2. Limited to facilities serving a maximum of 10 victims and may not be located within 300 feet of any other domestic violence shelter.</li> <li>3. Prohibited on the ground floor except residential uses located south of Baden Avenue, banks and walk-in offices which are subject to approval of a Use Permit.</li> <li>4. Subject to licensing requirements.</li> <li>5. Limited to upper stories unless at least 50 percent of the ground floor street frontage is occupied by food service uses.</li> <li>6. Must be located at least 1,000 feet from any other social service facility.</li> <li>7. Clinic uses may not occupy the ground floor along Grand Avenue, except on properties located west of Maple Avenue, which are subject to the approval of a conditional use permit.</li> </ol>							

**Figure 20.280.003**  
**Land Use Designations for Downtown Station Area Specific Plan Sub-Districts**



**20.280.004 Development Standards**

Tables 20.280.004-1 through 3 prescribe the development standards for the Downtown Station Area sub-districts. Additional regulations are denoted in the right-hand column. Section numbers in this column refer to other sections of this title, while individual letters refer to subsections that follow the tables, under **Section 20.280.005** “Additional Development Standards.”

**Table 20.280.004-1  
Lot, Density, and FAR Standards - Downtown Station Area Specific Plan Sub-districts**

<b>Standard</b>	<b>DTC</b>	<b>GAC</b>	<b>DRC</b>	<b>TORD</b>	<b>LCC</b>	<b>LNC</b>	<b>Additional Standards</b>
Minimum Lot Size (sq ft)	5,000	5,000	5,000	10,000	5,000	5,000	
Minimum Lot Width (sq ft)	50	50	50	50	50	50	
Minimum Lot Depth (sq ft)	n/a	n/a	80	n/a	80	n/a	
Floor Area Ratio (FAR)							
<i>Minimum FAR</i>	2.0	1.5	n/a	1.5	n/a	2.0	
<i>Maximum FAR</i>	6.0	3.0	3.0	2.5	n/a	3.0	Exclusive of structured parking
<i>Maximum FAR with Incentive Program</i>	8.0	4.0	3.25 (1)	3.5	n/a	n/a	Exclusive of structured parking
Residential Density (units per acre; included within FAR above)							
<i>Minimum Density</i>	80	14	40	n/a	20.1	40	
<i>Maximum Density</i>	100	60	80	n/a	40	60	
<i>Maximum Density with Incentive Program. Does not include density bonuses allowed per Chapter 20.390 Bonus Residential Density</i>	120 (A)	80 (A) / 100 (2)(A)	100 (A) / 125 (1)(A)	n/a	n/a	80 (A)	
<b>Limitations:</b>							
<ol style="list-style-type: none"> <li>1. For qualifying affordable Senior Housing projects</li> <li>2. For developments on corner parcels or lots greater than one (1) acre</li> </ol>							

**Table 20.280.004-2**

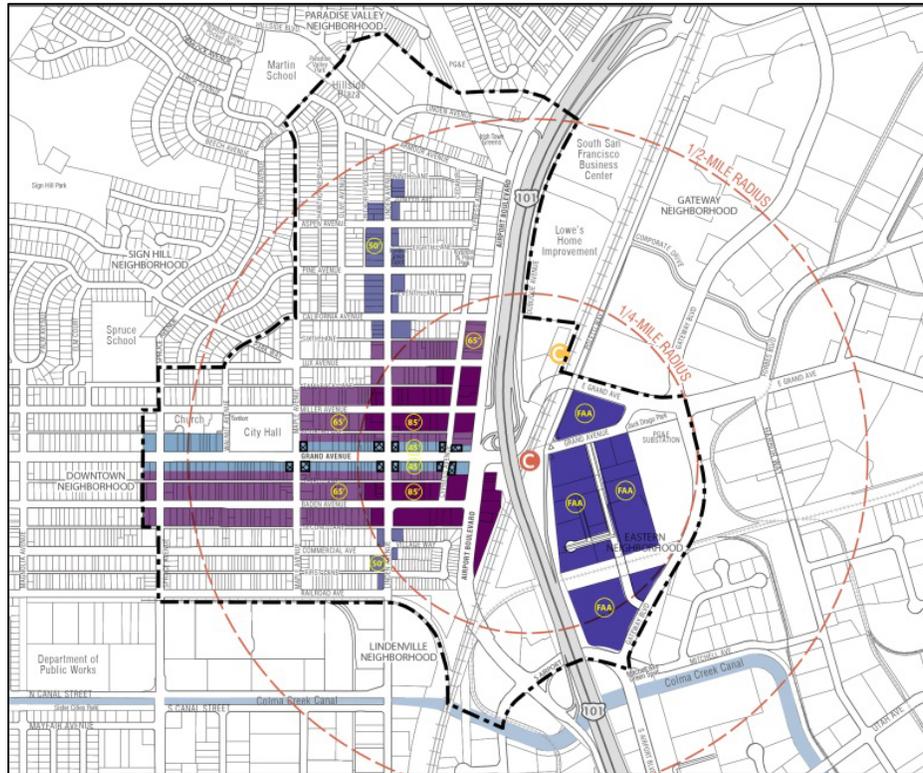
**Building Form and Location Standards – Downtown Station Area Specific Plan Sub-districts**

<b>Standard</b>	<b>DTC</b>	<b>GAC</b>	<b>DRC</b>	<b>TORD</b>	<b>LCC</b>	<b>LNC</b>	<b>Additional Standards</b>
Height (ft)							
<i>Maximum Building Height</i>	85	45-65 (1)(2)	65	FAA allowed	50	50	See Section 20.300 Height and Height Exceptions
<i>Minimum Ground Floor Height for non-residential uses</i>	15; 12 min clearance	15; 12 min clearance	15; 12 min clearance	15;12 min clearance	15; 12 min clearance		See above and Section 20.280.005(B)(1)
<i>Maximum Finished Floor Height (residential)</i>	5	n/a	5	n/a		5	See above
Yards (ft)							
<i>Grand Avenue (east and west) Frontage</i>	n/a	0	n/a	0		n/a	
<i>Pedestrian Priority Zone Street Frontage</i>	At property line or 10 feet from curb (whichever is greater)	n/a	At property line or 10 feet from curb (whichever is greater)	n/a		n/a	
<i>Eastern Neighborhood Streets except Grand Avenue Frontage</i>	n/a	n/a	n/a	20		n/a	
<i>Interior Side</i>	0; 10 when abutting residential district	0	0; 10 when abutting residential district	n/a		n/a	
<i>Rear</i>	0, 10 when abutting an R district (F)	0	20 (F)	10 for the first two stories, 15 thereafter (C)			
<i>Maximum Lot Coverage (% of lot)</i>	100	100	90	85	75	90	See Chap. 20.040 Rules of Measurement

**Limitations:**

1. Height break would occur a minimum of 30 feet from the front of the building
2. Corner properties may be exempt from this requirement, subject to evaluation by the decision-making authority in the review process and consistent with the Downtown Station Area Specific Plan design guidelines

**Figure 20.280.004-2  
Building Height**



**Table 20.280.004-3  
Open Space and Landscaping Standards - Downtown Station Area Specific Plan Sub-districts**

Standard	DTC	GAC	DRC	TORD	LCC	LNC	Additional Standards
Minimum Usable Open Space (sq ft per res. unit)	100	100	100	Refer to Section 20.280.00 7(K)		150	See Supplemental Regulations 20.100.004(D)(10)
Minimum Amount of Landscaping (% of site)	n/a	n/a	n/a	15		10	See Section 20.300.007 Landscaping

**20.280.005 Additional Development Standards**

**A. Increased Density and FAR Incentive Program.** An increase to the Maximum FAR or Maximum Density as referenced in Table 20.280.004-1 may be permitted for buildings with the approval of a Conditional Use Permit through the satisfaction of a combination of the following public benefits.

1. To be eligible for an increase to the Maximum FAR or Density incentives under this subsection, the public benefits that are included as part of a development project must demonstrate a positive contribution that is above and beyond the minimum required impact fees and other requirements of the particular project. The following preferences for public benefits to the Downtown community and the City may be considered as eligible to allow increased density and FAR standards for a project pursuant to this subsection:
  - a. Local Hire Program;
  - b. Public Art;
  - c. Funding or construction of local streetscape enhancements as identified in the Downtown Station Area Specific Plan;
  - d. Funding for enhanced public spaces;
  - e. Funding for public safety facilities, community meeting rooms, child care or similar;
  - f. Tenant space for local businesses or existing businesses in need of relocation;
  - g. Provision of green building measures over and above the applicable green building compliance threshold required pursuant to Title 15 (“Building and Construction”) of the South San Francisco Municipal Code;
  - h. Transit subsidy or other incentives for residents and/or employees; and
  - i. Other developer proposed incentives achieving a similar public benefit.
  
2. For projects seeking either an increase to the Maximum FAR or Maximum Density pursuant to this subsection, the following shall apply:
  - a. Applicant shall submit financial evaluation and analysis, information, and evidence to allow for a reasonable assessment of the value of the benefits offered relative to the incentives being sought, including the proposed public benefits as outlined above.
  - b. Applicant shall provide an assessment of the economic and/or intrinsic value of the proposed public benefit as compared with the economic value of the proposed development incentives requested by the applicant. The City may request an independent third party review, by a qualified appraisal expert, hired by the City at the applicant’s expense, to validate the valuation submitted by the applicant. This requirement is not intended to imply a need for the applicant to provide or disclose a complete project pro forma. Only the marginal costs of the proposed public benefit and incentive are required to be disclosed in the analysis.
  - c. Applicant shall provide an explanation as to the way in which the proposed amenities will further the City’s goals and objectives as outlined in the SSF Downtown Station Specific Plan, and conformance of the proposed project with the General Plan, Specific Plan provisions and Zoning Ordinance, and that a reasonable nexus exists between the public benefit provided and the incentive granted.

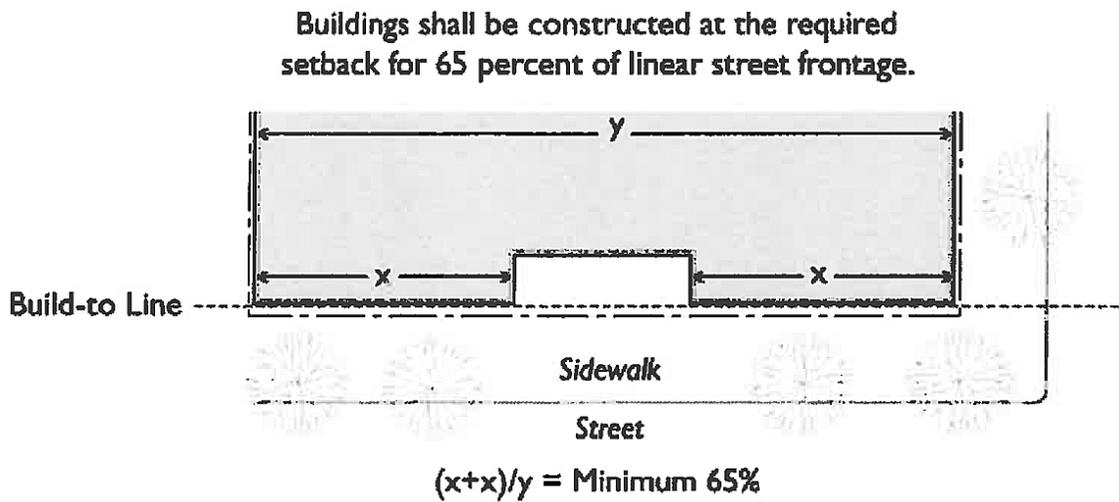
**B. Heights and Building Setbacks.**

1. **Ground Floor Height.** The minimum ground floor height for buildings with nonresidential uses at the ground level is 15 feet, with a minimum 12-foot clearance from floor to ceiling. For residential buildings, a ground floor garage may be exempt from this requirement, subject to evaluation by the decision-making authority in the review process.
2. **Finished Floor Height for Residential Uses.** The maximum finished floor height for ground floor residential uses is five feet above grade.

C. **Build-to Line.** Buildings shall be constructed at the required setback for at least 65 percent of linear street frontage. Build-to-Line criteria for locations within the sub-districts include:

1. Along the east and west extents of the GAC sub-district, no setback is allowed;
2. Within the Pedestrian Priority Zone, in the LNC sub-district, and in the LCC sub-district, buildings should be built to the property line or 15 feet from the curb, whichever is greater.
3. In the TO/RD sub-district on Sylvester Road and other new roads that may be constructed, setbacks up to 20 feet are allowed. These should be used to accommodate a primary building entry plaza, seating or signage, as well as generous site landscaping.
4. Standards pertaining to other DRC and DTC sub-districts apply as appropriate.

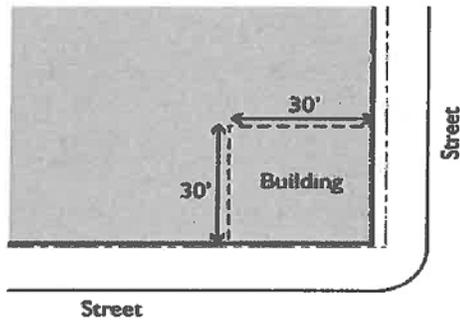
**Figure 20.280.004(C)**  
**Build-to Line**



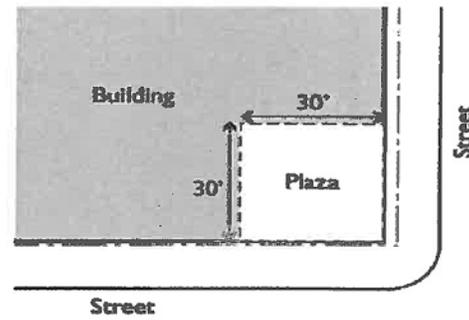
D. **Corner Build Area.** Buildings must be located in accordance with the required setbacks within 30 feet of every corner. Public plazas may be at the street corner provided buildings are built to the edge of the public plaza.

**Figure 20.280.004(D)**  
**Corner Build Area**

Buildings must be located in accordance with the required setbacks within 30 feet of every corner.



Public plazas may be at the street corner provided buildings are built to the edge of the plaza.



E. **Residential Usable Open Space.** A minimum of 100 square feet of usable open space is required per residential unit and may be provided as common or private open space, or a combination. Private areas typically consist of balconies, decks, patios, fenced yards, and other similar areas outside the residence. Common areas typically consist of landscaped areas, patios, swimming pools, barbeque areas, playgrounds, turf, or other such improvements as are appropriate to enhance the outdoor environment of the development; these can be in the form of courtyards at the ground level or terraces over parking podiums or on rooftops.

1. **Minimum Dimensions.**

a. **Private Open Space.** Private open space located on the ground level (e.g., yards, decks, patios) shall have no dimension less than 10 feet. Private open space located above ground level (e.g., balconies) shall have no dimension less than six feet.

b. **Common Open Space.** Minimum dimension of 20 feet.

2. **Usability.** A surface shall be provided that allows convenient use for outdoor living and/or recreation. Such surface may be any practicable combination of lawn, garden, flagstone, wood planking, concrete, or other serviceable, dust-free surfacing. Slope shall not exceed 10 percent.

a. **Accessibility.**

i. **Private Open Space.** The space shall be accessible to only one living unit by a doorway to a habitable room or hallway.

ii. **Common Open Space.** The space shall be accessible to the living units on the lot. It shall be served by any stairway or other access way qualifying as an egress facility from a habitable room.

F. **Private Storage Space.** Each residential unit shall have at least 200 cubic feet of enclosed, weather-proofed, and lockable private storage space with a minimum horizontal dimension of four feet.

G. **Required Parking.** Parking shall be required in accordance with Chapter 20.330 for Downtown Districts.

H. **Limitations on Curb Cuts.** Curb cuts shall be minimized and located where least likely to impede pedestrian circulation. Curb cuts shall be located at least 10 feet from any intersection curb return or pedestrian crosswalk.

I. **Truck Docks, Loading, and Service Areas.** Truck docks, loading areas, and service areas must be located at the rear or interior side of buildings and be screened so as not to be visible from public streets. Refer to Section 20.330.009 for specific requirements.

J. **Required Bicycle Parking.** Bicycle parking will be provided on-site where public bicycle parking on sidewalks or in plaza and park spaces is not available. A reduction in short-term bicycle

parking for commercial businesses will be considered based on contribution into a consolidated public bicycle parking amenity. Refer to Section 20.330.008 for specific requirements.

## 20.280.006 Supplemental Regulations - Downtown

### A. Required Active Frontage.

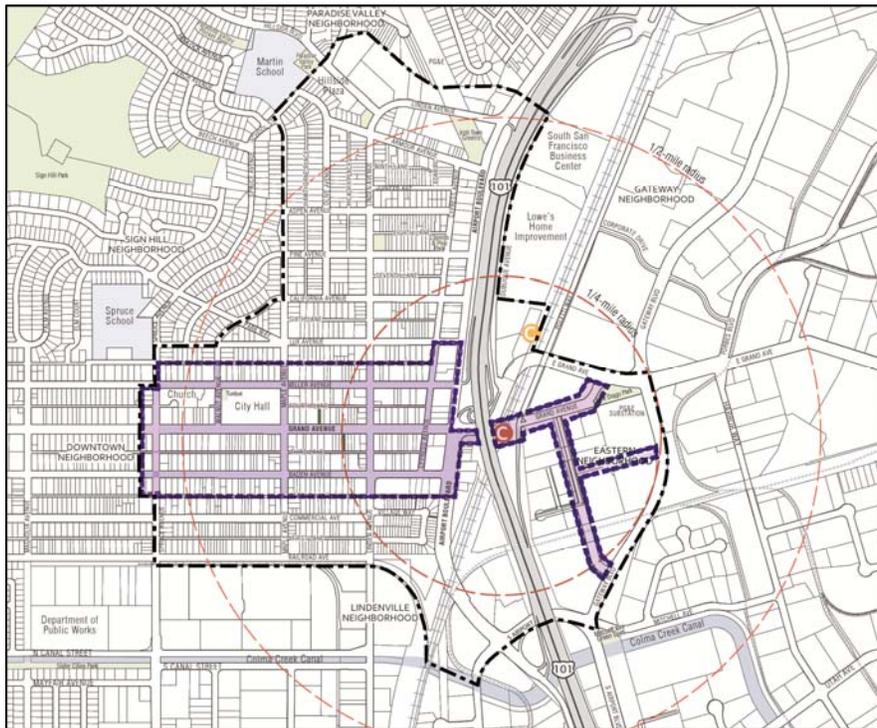
1. **Grand Avenue.** A minimum of 75 percent of the frontage of a site along Grand Avenue in the Downtown shall be devoted to active uses; in the Eastern Neighborhood a minimum of 35 percent of the frontage of a site along E. Grand Avenue shall be devoted to active uses.

2. **Pedestrian Priority Zone.** Properties within the Pedestrian Priority Zone, as shown in Figure 20.280.006(A), are encouraged to consider retail sales and/or eating and drinking establishment uses along the frontage of the site. Eastern Neighborhood streets besides Grand Avenue, such as Sylvester Road, are exempted from this requirement.

3. **Linden Avenue in the Linden Neighborhood Center.** A minimum of 65 percent of the frontage of a site along Linden Avenue in this area shall be devoted to active uses.

4. **Exceptions.** The Chief Planner may approve a reduction in these standards (not to exceed 25 percent of the standard) to allow for fire access, driveways, and for efficient site layout and site configuration. Exceptions beyond that are subject to Planning Commission approval.

**Figure 20.280.006(A)  
Pedestrian Priority Zone**



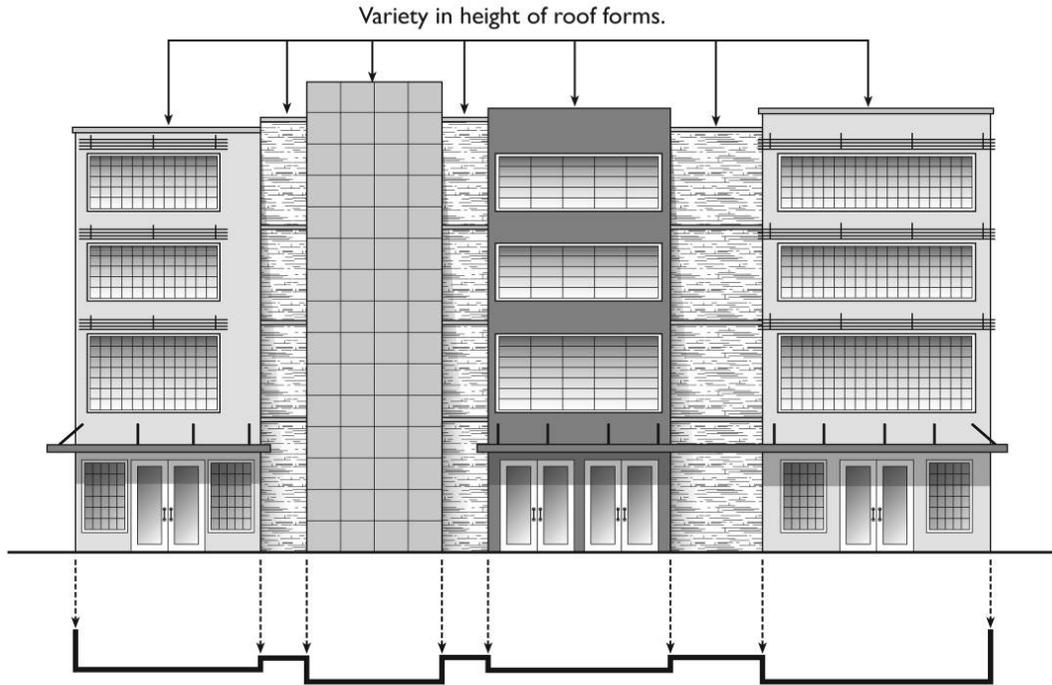
B. **Building Transparency and Required Openings.** Exterior walls facing and within 20 feet of a front or street side property line shall include windows, doors, or other openings for at least 60 percent of the building wall area located between 2.5 and 7 feet above the level of the sidewalk. No wall may run in a continuous plane for more than 20 feet without an opening. Openings fulfilling this requirement shall



**Figure 20.280.006(C)**  
**Architectural Articulation**

The following are examples of building design features that create architectural articulation.

*Variety in Wall Plane and Roof Form*



Exterior building walls vary in depth, through offsets, recesses and projections.

*Facade Design Incorporates Architectural Detail*



D. **Blank Walls.** Walls facing streets shall not run in a continuous plane for more than 20 feet without an opening. Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, display areas, sales areas, lobbies, or similar active spaces, or into window displays that are at least 18 inches deep.

1. **Exceptions.**

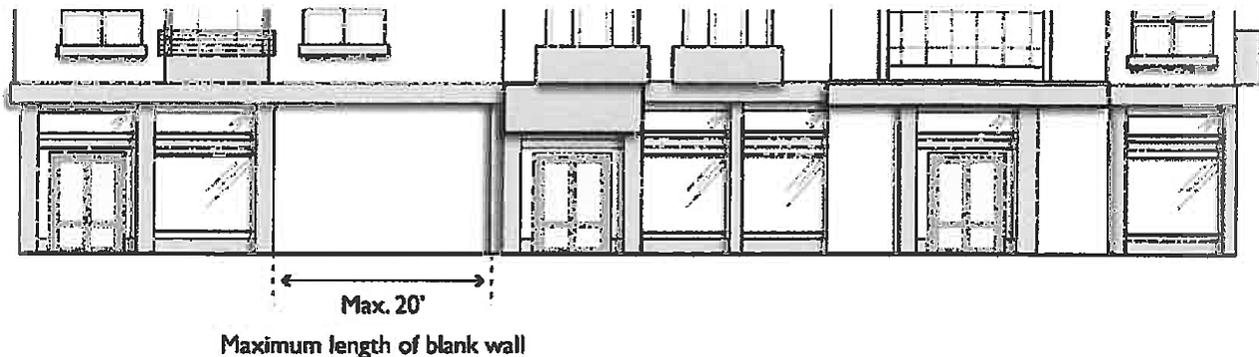
a. The maximum length of a blank wall may be 40 feet if it includes artwork approved by the City through the design review process as required by Chapter 20.480.

b. The maximum length of a blank wall may be 30 feet for retail establishments with a gross floor area of 25,000 square feet or greater.

c. The blank wall restrictions for a project may be reduced by the Chief Planner to address operational characteristics with which providing the required windows and openings is incompatible, such as in the case of a cinema or theater. Walls of street-facing buildings will exhibit architectural relief and detail, and/or will be screened with attractive landscaping, in such a way as to create visual interest at the pedestrian level.

**Figure 20.280.006(D)**

**Blank Walls**



E. **Exterior Building Materials and Colors.** Refer to the guidelines in Chapter 5 of the Downtown Station Area Specific Plan and consider the following:

1. A unified palette of materials shall be used on all sides of buildings and structured parking.
2. Use high quality, durable materials and finishes that provide a sense of permanence.
3. Give preference to sustainable materials, building systems and technologies.
4. Exterior materials may include stone, porcelain tile, brick, wood, stucco and other materials suited to commercial, mixed use, and residential construction.

5. In all cases, materials located at or near ground level should be high-quality, sturdy and visually interesting .

6. Materials must be approved by the City as part of the project review process.

F. **Building Orientation and Entrances.**

1. Buildings shall be oriented to face public streets. Residential development adjacent to public spaces or connections shall be oriented facing onto the public space.

2. Building entrances shall be emphasized with small entry plazas, vertical massing, and architectural elements such as awnings, arcades, or porticos.

3. Entrances located at corners shall generally be located at a 45 degree angle to the corner and shall have a distinct architectural treatment to create interest at the intersection and facilitate pedestrian flow around the corner. Different treatments may include angled or rounded corners, arches, and other architectural elements. All building and dwelling units located in the interior of a site shall have entrances from the sidewalk that are designed as an extension of the public sidewalk and connect to a public sidewalk.

4. In residential mixed-use developments, entrances to residential units shall be physically separated from the entrances to the commercial uses and clearly marked with a physical feature such as a recess or projection incorporated into the building or appropriately scaled element applied to the façade.

5. All ground floor residential units shall have the primary entrance, either individual or shared, facing the public street or a pedestrian connection and shall incorporate a projection (e.g., porch or stoop) or recess at least 40 square feet in area, with a minimum depth of 5 feet. Alternative entry designs that cface the street, such as a trellis or a landscaped courtyard entry, may be approved by the Chief Planner or Design Review Board.

**G. Unbundling Parking from Residential Uses.** For residential condominium or other multi-family ownership projects, parking in excess of one space per unit may be sold or rented separate from the residential unit. For apartment developments, 50 percent of the required parking may be unbundled. All spaces shall be reserved for residential tenants within the development.

**H. Limitations on Location of Parking.**

**1. General**

a. Share access drives and cross access easements to parking facilities wherever feasible in order to minimize curb cuts and potential conflicts with pedestrians

b. Minimize the number of vehicular access points from the following streets to reduce the total number of curb cuts:

- i. Miller Avenue
- ii. Baden Avenue
- iii. Linden Avenue

c. No curb cuts shall be allowed along Grand Avenue unless no other access is feasible:

**2. Surface Parking Lots**

a. Locate surface parking lots away from street edges or behind buildings and provide decorative, landscaped, or other screening.

b. Landscape a minimum 5 feet perimeter setback area around parking lots.

**3. Private or Shared Garages**

a. Organize at-grade garages for lower density residential development (i.e., rowhouses, townhouses) in well-landscaped parking lanes and parking courts leading to individual garages.

**4. Parking Structures**

a. Parking structures should be located away from primary pedestrian walkways, unless otherwise approved by the Chief Planner..

b. Pedestrian entries and stairwells for parking structures should be located adjacent to public streets and along major pedestrian connections.

c. Pedestrian entries should be located to minimize conflicts between pedestrians, bicycles and vehicles.

**I. Maximum Block Length.** Existing block configurations shall remain intact. Blocks shall not be consolidated. Wherever possible, mid-block pedestrian connections and alleys are encouraged especially where blocks exceed 300 feet in length.

### **20.280.007 Supplemental Regulations – Eastern Neighborhood**

A. **Building Bulk.** Buildings in the Eastern Neighborhood must be designed to avoid a large-scale, bulky or monolithic appearance. This may be accomplished through building set-backs at upper levels, through changes in materials or color, or other means (See subsection D. below).

B. **Required Active Frontage.**

1. **E. Grand Avenue.** In the Eastern Neighborhood a minimum of 35 percent of the frontage of a site along Grand Avenue shall be devoted to active uses.

2. **Exceptions.** The Chief Planner may approve a reduction in these standards (not to exceed 25 percent of the standard) to allow for fire access, driveways, and for efficient site layout and site configuration. Exceptions beyond that are subject to Planning Commission approval.

C. **Building Transparency and Required Openings.** Exterior walls along Grand Avenue in the Eastern Neighborhood shall include windows, doors, or other openings for at least 60 percent of the building wall area located between 2.5 and 7 feet above the level of the sidewalk. No wall may run in a continuous plane for more than 20 feet without an opening. Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, sales areas, lobbies, or similar active spaces, or into window displays that are at least 18 inches deep. They shall not provide views into parking or vehicle circulation areas.

1. **Exceptions.** The building transparency requirement for a project may be reduced by the Chief Planner to address operational characteristics with which providing the required windows and openings is incompatible. Walls of street-facing buildings will exhibit architectural relief and detail, and/or will be screened with attractive landscaping, in such a way as to create visual interest at the pedestrian level.

D. **Architectural Articulation.** Buildings shall include sufficient architectural design features to create visual interest and avoid a large-scale, bulky or “box-like” appearance. Different ways that this requirement may be met include, but are not limited to those listed below; compliance with this requirement shall be evaluated by the decision-making authority in the review process. Refer to the guidelines in Chapter 5 of the Downtown Station Area Specific Plan and consider the following:

1. **Variety in Wall Plane.** Exterior building walls vary in depth and/or direction. Exterior walls exhibit offsets, recesses, or projections with significant depth, or a repeated pattern of offsets, recesses, or projections of smaller depth.

2. **Variety in Height or Roof Forms.** Building height is varied so that a significant portion of the building has a noticeable change in height; or roof forms are varied over different portions of the building through changes in pitch, plane, and orientation.

3. **Façade Design Incorporates Architectural Detail.** The building façades incorporate details such as window trim, window recesses, cornices, belt courses, changes in material, or other design elements in an integrated composition. The use of materials, textures, and colors enhance architectural interest and emphasize details and changes in plane. Some of the architectural features of the front façade are incorporated into the rear and side elevations.

E. **Blank Walls.** Walls facing Grand Avenue in the Eastern Neighborhood shall not run in a continuous plane for more than 20 feet without an opening. Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, display areas, sales areas, lobbies, or similar active spaces, or into window displays that are at least 18 inches deep. On streets other than E. Grand Avenue, ground level facades should generally be glazed with the ability to see into lobbies, offices, cafeterias, or other active uses.

1. **Exceptions.**

a. The maximum length of a blank wall may be 40 feet if it includes artwork approved by the City through the design review process, as required by Chapter 20.480

b. The maximum length of a blank wall may be 30 feet for retail establishments with a gross floor area of 25,000 square feet or greater.

c. The blank wall restrictions for a project may be reduced by the Chief Planner to address operational characteristics with which providing the required windows and openings is incompatible, such as in the case of a cinema or theater. Walls of street-facing buildings will exhibit architectural relief and detail, and/or will be screened with attractive landscaping, in such a way as to create visual interest at the pedestrian level.

F. **Exterior Building Materials and Colors.** Refer to the guidelines in Chapter 5 of the Downtown Station Area Specific Plan and consider the following:

1. A unified palette of materials shall be used on all sides of buildings and structured parking.

2. Use high quality, durable materials and finishes that provide a sense of permanence.

3. Give preference to sustainable materials, building systems and technologies.

4. Exterior materials in the Eastern Neighborhood may include those suited to mid- and high-rise construction, including metal panels, glass curtain walls, and stone or similar wall systems.

5. In all cases, materials located at or near ground level should be high-quality, sturdy and visually interesting.

6. Materials must be approved by the City as part of the project review process.

G. **Building Orientation and Entrances.**

1. Buildings should generally be oriented such that the primary building entry faces the public street. Secondary entries, such as those from a parking structure or from a pedestrian/service alley, may be on an alley or at the rear of the building.

2. Building entrances shall be emphasized with entry plazas, special landscaping, vertical massing, and architectural elements such as canopies, arcades, or porticos.

3. Entrances should generally not be located at corners but rather in a middle portion of the building.

H. **Limitations on Location of Parking.**

1. Parking in the Eastern Neighborhood is encouraged to be provided with parking structures, limiting onsite parking lots.

2. Parking structure access stairs must directly access a sidewalk or pedestrian alley and must be well-lit and secure.

3. Parking access shall be provided from a side street or alley wherever possible. Curb cuts shall be minimized and located in areas least likely to impede pedestrian circulation.

4. Surface parking shall be located behind buildings or at the side, but not in front of a building along the street. Where lots are located at the side of a building, screening through fencing, hedges or other plantings will be employed to screen cars from view.

5. Surface parking may not be located within 40 feet of a street facing property line

6. Surface parking lots and individual spaces must be separated from on-site buildings by a minimum distance of six feet, which may be occupied by landscaping or required walkways.

I. **Maximum Block Length.**

1. 300 feet is the preferred block length. Maximum block length is 600 feet.

2. Where blocks exceed 300 feet in length, mid-block alleys or pedestrian lanes must be provided to allow more convenient movement by pedestrians and bicyclists.

J. **Pedestrian Access.** On-site pedestrian circulation and access must be provided according to the following standards.

1. **Internal Connections.** A system of pedestrian walkways shall connect all buildings on a site to each other, to on-site automobile and bicycle parking areas, and to any on-site open space areas or pedestrian amenities.

2. **To Street and Open Space Network.** Regular connections between on-site walkways and the public sidewalk, public open space, and other pedestrian areas shall be provided.

3. **To Transit.** Safe and convenient pedestrian connections shall be provided from transit stops to building entrances. Sidewalk “bulb-outs” or bus “pullouts” may be required at potential bus stops.

4. **Interior Pedestrian Walkway Design.**

a. Walkways shall be a minimum of five feet wide, shall be hard-surfaced, and paved with permeable materials.

b. Where a required walkway crosses driveways, parking areas, or loading areas, such crossing must be clearly identifiable through the use of a raised crosswalk, a different paving material, or similar method.

c. Where a required walkway is parallel and adjacent to an auto travel lane, it must be raised or separated from the auto travel lane by a raised curb at least four inches high, bollards, or other physical barrier.

K. **Public Open Space.** For new development in the Eastern Neighborhood on lots larger than 15,000 square feet, a minimum of 5 percent of the lot shall be set aside for open space. Such open space shall be usable by the public and employees of the development. Such open space shall be located on the primary street frontage of the lot and shall be in addition to any courtyard or other building-related open spaces. These public open spaces may take the form of entry plazas or seating areas, such as near transit stops, as a way of creating activity along the street, encouraging pedestrian use, and providing amenities for the district. No dimension of the open space shall be less than 20 feet.

#### **20.280.008 Design Review**

All development shall be subject to design review, pursuant to Chapter 20.480 (“Design Review”). Design guidelines for the Downtown Station Area Specific Plan District, adopted as part of the Downtown Station Area Specific Plan, shall be used.

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